

4. URBAN MORPHOLOGY

The following chapter consists of an assessment of built character, looking at character areas, typical block typologies, and emerging characters.

CHARACTER AREAS AS PER NEWHAM CHARACTER STUDY, 2017

The character areas below are taken from the Newham Character Study, 2017. Seven character areas are illustrated, each highlighting broad character types and differentiations across the borough, relating primarily to built character.

The largest of the 7 includes Upton, East Ham, West Ham, Plaistow which consists of predominantly terraced housing and domestic typologies and uses.

The Olympic Legacy consists of predominantly large sporting institutions sizable new parks and contemporary and high density housing with retail use around Stratford Station.

The River Lea is characterised by industrial uses along the river, while Canning Town comprises residential use with some emerging high density retail and commercial centres. Beckton is unique in its street layout, forming a curved grid with a cul-de-sac layout as the framework to a suburban residential neighbourhood.

Gallions Reach provides little residential use currently, with its dominant function consisting of large, institutional industrial infrastructure and out of town retail.

The Royal Docks is an area that is rapidly evolving from an industrial area to a place that consists of high density residential use, hotel and commercial, alongside the existing City Airport.



CURRENT AND EMERGING BUILT CHARACTERS

Reviewing Character Areas

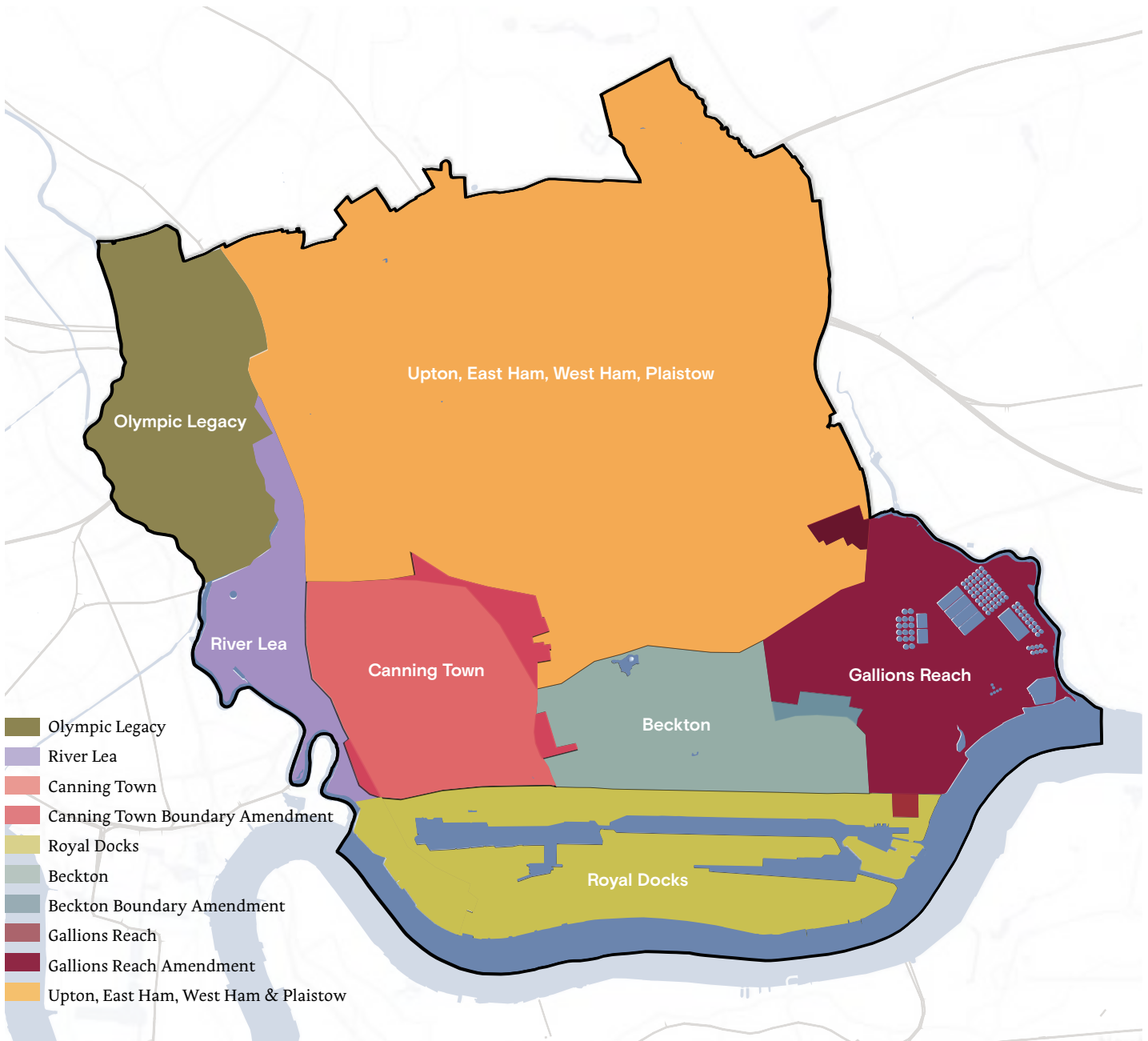
Having considered and investigated the character areas as outlined in the Newham Character Study, 2017, considerations of the edges have emerged that could be considered as an adaptation of the character area boundaries, taking into account emerging character shifts with new development. As a categorisation of built form, the number of areas works well overall, and any sub-splitting would result in significant fragmentation.

The lighter areas highlighted in the map below are representative of the character area boundary (2017) alterations concluded from this study. The areas with higher saturation, represent adjustments to the original boundary positions, following a review of the study.

Analysing Character Areas

The following pages describe the urban characteristics of each area, including justification for the amended boundary positions. These cover Typology; Morphology; Urban structure; Green space; Boundary condition; and Density.

Looking forward to considerations of neighbourhood mapping for the purposes of this study, these character areas offer perhaps too broad a designation. In some cases it is also likely that built character will transcend neighbourhood boundaries as further sub-division will be necessary based on factors beyond built form. A consideration of functional character in the next chapter offers another layer of consideration for this.



OLYMPIC LEGACY

The Olympic Legacy character area consists of predominantly new residential development, with other social amenity buildings such as libraries, churches and schools, and local and town centres located along main roads. Queen Elizabeth Olympic Park dominates a large portion of the character area to the north of Pudding Mill Lane station, surrounded by quality parks and green space on either side of the River Lea, Waterworks River and City Mill River.

The dominant built form consists of mid-rise and tall buildings of medium-density residential, hotel and office use. Most of the contemporary buildings directly interface with the street with no setback, or have a small planted hedge. The northern edge of the character area has some plots of post-war housing estates. Queen Elizabeth Olympic Park and related legacy buildings provide a major high-value urban leisure space and tourist destination whilst also improving connectivity across the river. The Greenway provides a green link towards Beckton, and the river corridors form the basis of Lea River Park / Leaway to the Royal Docks and providing connections to Bromley-By-Bow and Poplar.

The character area itself is subdivided into a series of sub-areas that reflect its varied characteristic and geography. These include North Stratford and Eton Manor; Central Stratford and Southern Queen Elizabeth Olympic Park; and Pudding Mill, Sugar House Lane and Mill Meads (see LLDC Characterisation Study, 2018 for more information).

North Stratford and Eton Manor is characterised by a well defined urban structure, consisting of a gridded street pattern and perimeter blocks. Blocks comprise linear blocks, townhouses and terraces organised around central courtyards set within a pedestrian-centric, verdant public realm. Central Stratford and Southern Queen Elizabeth Olympic Park is a highly diverse area comprising a number of different urban structures and architectural styles. Key components include the major rail infrastructure; big box retail environment of Westfield Stratford City; the high density and large footprint mixed use buildings along the International Quarter and Stratford Waterfront; and the free form layout of Carpenters Estate. Pudding Mill, Sugar House Lane and Mill Meads is defined by its relationship to heritage assets including Grade 1 Listed Tide Mill at Three Mills; and the Grade 1* Abbey Mills Pumping Station. Recent development at Pudding Mill and Sugar House Lane has seen the emergence of a mid-density residential character, defined by courtyard perimeter blocks on formerly industrial land.



New gridded street network around Honour Lea Avenue.



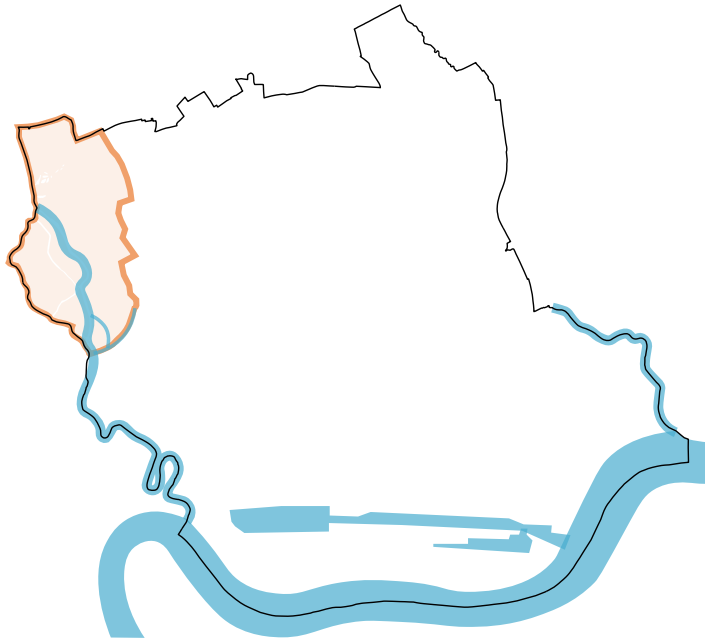
Contemporary residential development in Chobham Manor.



Carpenters Estate is characterised by a free form layout of linear blocks and urban terraces.



The International Quarter is a high density, mixed use environment characterised by tall, large footprint buildings.



Queen Elizabeth Olympic park surrounding London Stadium.



Three Mills Conservation Area is home to a cluster of Listed buildings including Grade 1 Tide Mill and Grade 11 Clock Mill.

- LB Newham Character Area Boundary
- Hard / defined Character Edge Condition
- Soft Character Edge Condition

Character Area Edges and Definition

The edges of the character are well defined. The northern and western edges of the character area is clearly defined by the edge of the borough. This edge continues to be defined by the A112 along the north eastern edge and is then defined by the railway line running north-south through Stratford. The southern edge of the character area is defined by the edge of the Channelsea River and Abbey Creek.

Dominant typology

- Tall buildings of medium-density residential, hotel and office buildings.
- Contemporary buildings with some balconies overlooking the street.
- Buildings directly interface with the street with no setback, or have a small planted hedge.
- Streets designed with generous pavement, trees and planted areas between parking bays.
- Gridded street layout



Honour Lea Avenue.

Urban Neighbourhood

Secondary typology

- Tall buildings and high rise residential towers with some buildings being set above a plinth.
- Buildings are either set directly interfacing the street with windows but no active frontages or are set back behind a taller brick wall.
- The road is wide, approximately 40m between building façades and produces a poor pedestrian environment.
- Identified in part as a high street by the London Plan.



Corner of High Street and Abbey Lane.

Urban Neighbourhood

Tertiary typology

- Historic terraced housing.
- Arranged in terraced perimeter blocks of narrow, at modestly high densities (5m frontage).
- Houses are situated on long slender plots consistent with the width of the frontages.
- A sense of composition achieved through the application of repeated building styles and uniform front boundary treatments.
- Houses are set directly interfacing the street with no setback.
- Modest decorative elements on the facade.

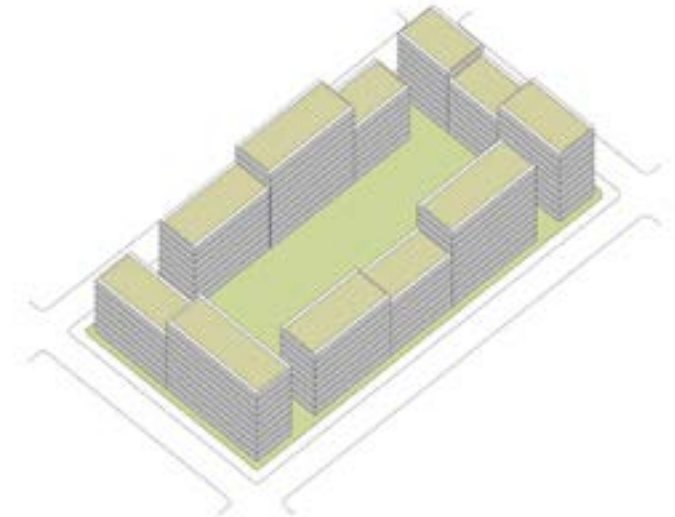


Wise Road.

Historic Inner Suburb

Summary

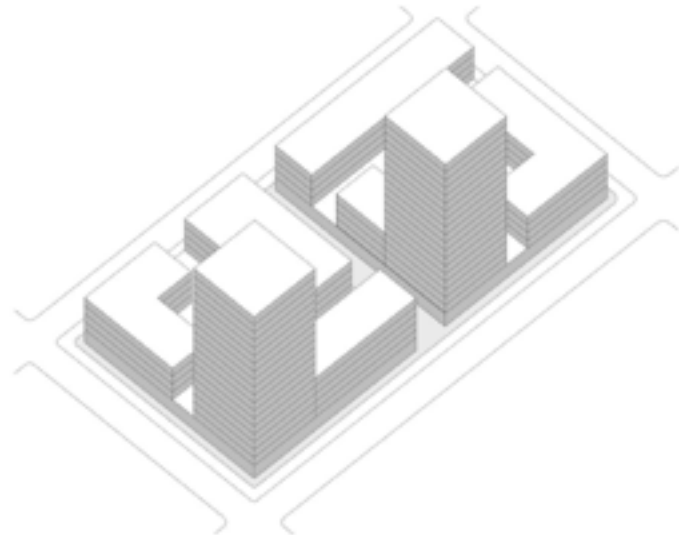
- Average. 6-10 storeys.
- Medium to high density
- Perimeter block development
- Central, shared, green courtyard
- Flat roofscape with greening
- Contemporary architecture
- Approximately net 3.5 FAR.



Typical residential block

Summary

- Average in excess of 18 storeys.
- High density
- Tall buildings
- Flat roofscape with some green roofs
- Late 1990s architecture
- Approximately net 4.0 + FAR (some examples show a considerably higher FAR, responding to the tall extrusion of almost the entire site)



Typical tall building

Summary

- 2 storeys
- Low to medium density
- Domestic architecture
- Pitched roofscape visible from the street
- Central private gardens
- 1830s 1900s architecture
- Approximately net 1.0 FAR



Typical houses

Current and emerging urban characters

The area has developed and changed rapidly since the development of Queen Elizabeth Olympic Park and its surrounds. This has catalysed continuing investment in residential and other uses in the area. An increasing typology of contemporary, mid-rise and tall residential and office buildings are emerging around Stratford City, north of Stratford town centre, and Stratford High Street, where there has been some redevelopment of former Council housing and gap sites. These development typically include retail and business units on ground floors.

The most recent development has included estate-based renewal, redeveloping high rise blocks and high density housing. There has also been some infill development of former railway and industrial sites, with tall residential development.

The parks and public space as part of the Olympic Park development has also increased the areas' qualitative and financial value, with good access to green and public space making it a more attractive place to live.



Contemporary and mid-rise residential apartments



New commercial centre around Stratford International station.



New quality green and public space around the Olympic Stadium.

RIVER LEA

The River Lea character area is characterised by riverside industrial development together with large scale infrastructure. The area has been essential for London's economic development. The Lea River's abundant flow of water it has, over time, worked mills, transported products, and received merchandise from everywhere. The majority of the character area is largely inaccessible to the public owing to the number of gated industrial estates. The Leaway does provide access along the water's edge and will soon connect from Cody Dock to Canning Town, a critical link improving north-south connectivity.

The built form consists of post-war and modern industrial park development, comprising industries located in sheds and offices of varying sizes, usually constructed in concrete and steel. The newer industrial units are constructed within gated industrial estates, with planted and private streets, leaving large areas of impermeable plots. There is also small scale industry and recycling processing between the river Lea and Bidder Street.

The area has substantial remnants of historic industry—former railway land, river quays, Bow Locks and the Grade II listed gasholders at the former Bromley-by-Bow gasworks site in the north of the area (Grade II listed, 1872). The gas holders are dominant feature in the flat urban landscape, itself defined by its relationship to the meandering waterway and creekside landscape.

A few quality parks are located in the area, namely; West Ham Memorial Gardens is located to the south; Three Mills Park (Conservation Area); and Abbey Lane Open Space. A portion of the Greenway runs through the area along Abbey Lane open Space. The edge of the Bow Creek, has a pedestrian and cycling route along its eastern edge.

City-scale roads and rail infrastructure crosses the River Lea and character area forming the principal urban structure. These also creates north-south barriers to movement along the riverside. The River Lea is navigable but largely unused and its edges are well planted, forming a natural green barrier between the water and the road. The DLR and Jubilee Line run north south and, with the river and M11 Link Road further west, create barriers to east-west movement. Stephenson Street and Cody Road are the two main roads through the area, with all roads off this main structure forming entrances to private industrial estates, producing an illegible an impermeable network of streets.



Grade II listed Bromley-by-Bow gasworks



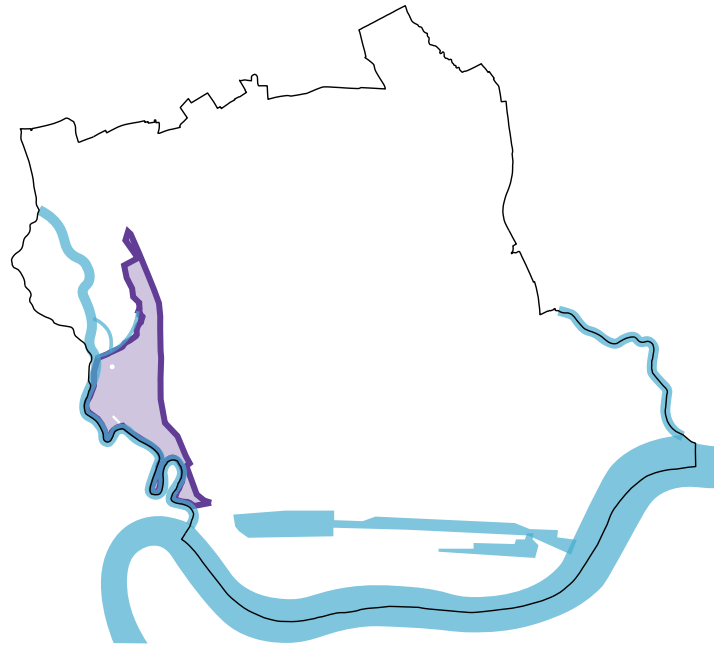
Gated entrance to a large private industrial estate on the western end of Cody Road.



Typical shed landscape with some quality paving and planted trees and edges along the pavement.



Cody Road is well planted on both sides of the streets, minimising views to industrial estates on either side.



- Character Area Boundary
- Hard Character Edge Condition
- Soft Character Edge Condition

Character Area Edges and Definition

The edges of the character are well defined. The western edge of the character area is defined by the edge of the borough boundary. To the east, the railway line and A1011 forms a defined barrier between industrial and residential development.



Cycling and walking route along Bow Creek with industrial development to the right.



Metal waste site and overhead pylon - Ives Road



Illegible network of streets leading to private industrial estates.

Dominant typology

- Older industrial warehouses.
- Low density industrial buildings.
- Typical uses include some retail and warehouse space.
- Buildings are set back from the street to allow off street, perpendicular parking on the pavement.
- Some gated warehouse estates.
- Poor pedestrian and cycling environment.



Industrial

Secondary typology

- Industrial infrastructure
- Aging or unused infrastructure on 2 large sites across the character area.
- Visually permeable fencing with some high security electric fencing around entire site.
- Little to no pavement around the character area produces a poor public environment.



Industrial

Current and emerging urban characters

Older industrial site in the area have been renewed and upgraded with no change in use. An example of this is the 'ProLogis Park' development, spurred on by improvements to the local infrastructure such as the DLR, and bridge connections over the Lea River.

There are some proposals for large scale high density mixed use schemes on the former Parcelforce site in the north and the strip between rail lines and Silvertown Way in the south.

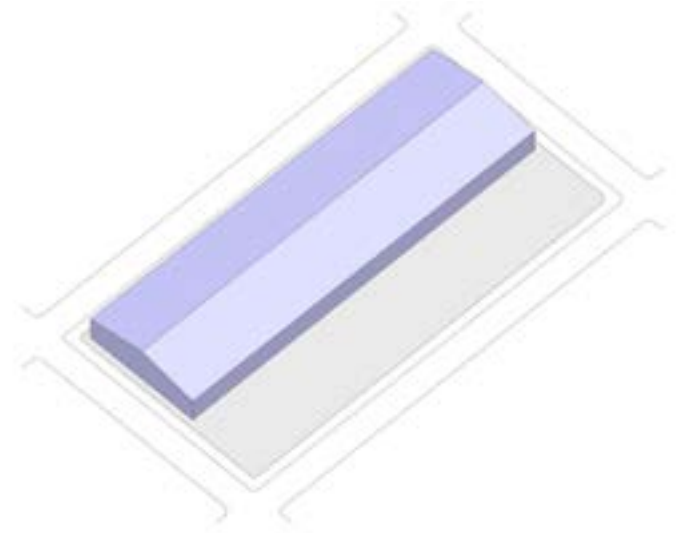
New development west of West Ham station is anticipated, as is regeneration around Bidder Street.



Upgraded ProLogis park development with bridge connection over the River Lea.

Summary

- Single storey
- Low density
- Single large building on a plot.
- Car parks in the front or rear of building.
- Approximately net 0.5 FAR



Typical warehouse block

Summary

- Collection of warehouse buildings and infrastructure.
 - Low density
 - Large, impermeable plot.
 - Car and truck parking within the sites.
 - Approximately net 0.4 FAR
-

CANNING TOWN

A key feature of the Canning Town character area includes terraced residential neighbourhoods, with some large scale infrastructure such as the Canning Town flyover, and some notable public buildings including churches and libraries. Post-war residential development produces the dominant built form, with 1960s / 70s redevelopment across large areas.

Barking Road is a primary road in the area and runs through the Canning Town Centre. The town centre contains some rows of 5-6 storey historic and inter-war development of flats and offices above shops, and tall contemporary office, commercial and residential buildings with some towers along Phoenix Way. Parts of Barking Road in this area are also distinguished by grade II listed buildings - the former Canning Town library, and the adjacent Public Hall.

The street grid in the area is inconsistent with many cul-de-sacs that break the street transparency. The lack of a clear block structure, accommodating older and new layouts is disrupted by infrastructure barriers. There is an unclear definition of fronts, backs and public and private spaces, and a poor relationship between buildings and street spaces as many post-war housing developments departed from a clear relationship with the street in favour of internal courts.

The Victorian St Luke's Church has a strong presence, set within its immediate grounds and fronting onto Kier Hardie Recreation Ground. It is sheltered from the busy A1011 road through new development that steps up in scale and mass towards the road, albeit sensitively concentrated away from the church to allow it (and its setting) 'breathing space'.



Barking Road with ground floor retail.



Former Canning Town library



Canning Town Public Hall.



Typical historic residential street layout with row houses face the street in a perimeter block with private gardens in the centre of the block.



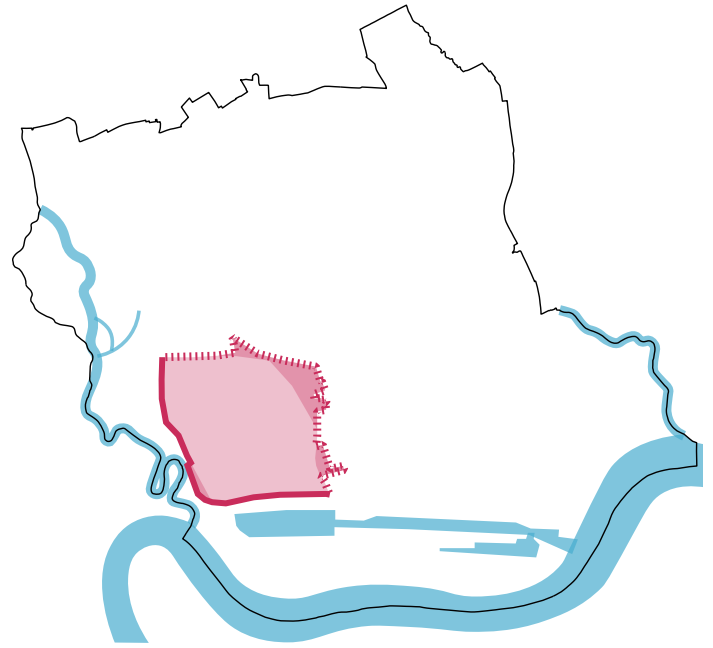
Typical historic terraced residential housing.



Silvertown Way viaduct



Typical post-war residential development



- Character Area Boundary
- Proposed Extension to Character Area Boundary
- Hard Character Edge Condition
- Soft Character Edge Condition

Character Area Edges and Definition

The western and eastern edge of the character area are clearly defined by Manor Road and Victoria Dock Road (and the railway line on this edge). The northern edge is defined partly by the Greenway and the East London Crematorium and memorial Recreation ground. We have extended the character area northwards to meet the Greenway and eastwards to Prince Regent Lane, with small pockets extending across the road. This amended boundary now takes account of the repetition of Historic Inner Suburbs of compact urban terraces and more spacious inter-war suburban terraces that characterise the area, which previously sat outside of the boundary. The northern and eastern edges are illustrated as soft character edge conditions as the urban character on either side of these boundaries are not too dissimilar. Either side of Prince Regent Lane is characterised by historic terraced housing, although the streets grids differ.

Dominant typology:

- Historic terraced housing.
- Consistent 6m wide facade.
- Buildings have small setback from the street with a stoop area behind a low brick wall.
- Large bay windows face the street.
- Some decorative elements on the facade of the buildings.
- Materials consists of a consistent brick facade.
- All houses have a back garden.
- Gridded street layout.



Cambus Road.

Historic Inner Suburb

Secondary typology

- Unembellished brick walk-up flats.
- Housing – reflects a period of fast-paced state-led systematic development.
- Buildings are set back from the street with a high impermeable fencing, clearly delineating public and private space.



New Barn Street.

Post War Inner Suburb

Tertiary typology

- Mix of contemporary residential and office buildings and older, historic residential flats with ground floor retail.
- Mix of buildings heights and uses.
- Buildings are set directly interfacing the street, but often does not include an active front.
- Some balconies on residential buildings.
- Buildings are front with perpendicular parking at its base, on the street.
- Linear high street context



Barking Road.

Town Centre

Summary

- 2 storeys
- Low to medium density
- Domestic architecture
- Pitched roofscape
- Perimeter block development
- Central private garden
- 1830s 1900s architecture
- Approximately net 1.0 FAR



Typical houses

Summary

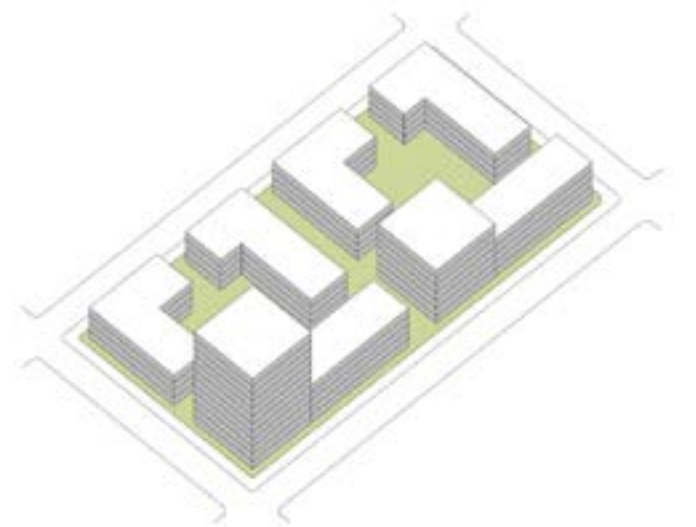
- Average 2-3 storeys
- Low to medium density
- Flat roofscape
- Perimeter block development
- Central private garden
- 1960s / 70s redevelopment.
- Approximately net 1.0 FAR.



Typical residential block

Summary

- Average 3-12 storeys
- Medium density
- Flat roofscape
- Tall buildings with plinth
- Contemporary development.
- Approximately net 2.5 FAR.



Typical mixed-use block

Current and emerging urban characters

New development in the area comprises of residential towers and vertical mixed use town centre around pedestrianised streets.

The Canning Town area is undergoing extensive regeneration guided by a masterplanning process to provide new housing, extend the town centre and main streets linking Canning Town and Custom House — a 'residential street', and an 'activity street' running through parkland.

The A13 / Silvertown Way roundabout has been removed and the road layout adjusted to allow for reduced traffic and extensively improved public realm experience for pedestrians and cyclists.

The town centre along Barking Road has seen redevelopment and has delivered high quality residential accommodation and a renewed Rathbone Market space with tall office space.



New residential and office development on Barking Road town centre



Emerging tall and contemporary buildings in the town centre between the A13 and Barking Road



New development at the Rathbone Market

ROYAL DOCKS

The Royal Docks character area is rapidly changing and redeveloping from its past industrial character to a mixed-use residential one, with various forms of residential typologies existing within the character area. The majority of the edges of the character area interfacing Gallions Point Marina consists of industrial development with the London City Airport at its centre. The international convention centre, ExCeL is located between Royal Victoria Dock and railway line running parallel to Victoria Dock Road. The University of East London is located on the northern edge of the Gallions Point Marina.

The western and eastern edges of the southern edge of the character area interfacing the Thames River consists of residential buildings with pedestrian and cycling walkways along the river. This route is separated by the older industrial development such as the Tate & Lyle sugar factory interfacing the river at the southern edge of the study area. The highways and major routes that run across the site such as Victoria Docks, Royal Albert way, North Woolwich Road, Albert Road and Connaught Bridge create barriers to pedestrian and cycling movement and create distinct characters between them that are not easily connected.

The housing typologies in the areas include post-war housing such as Cyprus and Britannia Village, which consist of 2-4 storey residential development with private ground floor gardens in the centre of a perimeter block layout.

The North Woolwich area consists of tower blocks, walk-up blocks and terraces with perpendicular on street parking and parking garages directly interfacing some streets.

Contemporary developments including flats of 4-6 storeys, towers of around 20 storeys, and 12-storey tall blocks around Barrier Park. Higher quality streets with quality landscaping and public spaces exist around this area.

Some public parks are dotted across the character area on the Thames River edge. Royal Victoria Gardens is located to the east, and Lyle Park to the west and the most contemporary park associated with comprehensive redevelopment projects is at Thames Barrier Park. There are a number of statutory listed buildings in the area such as the Connaught Tavern and Gallions Hotel, the Stothert and Pitt Cranes, Warehouses W and K and the entrance to Woolwich Pedestrian Tunnel.



© Google maps
Britannia Village



© Google street view
Residential towers with parking streets.



Contemporary office and industrial buildings with a central green public space.



ExCeL London is a metropolitan scale conference destination.



Residential buildings interfacing the Thames River with a walking and pedestrian route along the river.



Tate & Lyle sugar factory between the Thames River and Albert Road.



- Character Area Boundary
- Proposed Subtraction from LB Newham Character Area Boundary
- Hard Character Edge Condition

Character Area Edges and Definition

The Royal Docks is a well defined character area due to its unique land use heritage. The A1020 defines a hard character edge between the Royal Docks and residential areas in the north. A land parcel to the east has been removed since it contains active large industry in a SIL designation, relating better to the coarse grain, industrial character of Gallions Reach.



Emerging contemporary street network and residential areas around Admiralty Avenue.

Dominant typology

- Older industrial warehouses.
- Low density industrial buildings.
- Logistics and storage warehouse space.
- Buildings are set back from the street to allow off street, perpendicular parking and storage on the pavement.
- Poor pedestrian and cycling environment.



Thames Road.

Industrial

Secondary typology

- Mixed residential apartments and houses..
- Housing – reflects a period of fast-paced state-led systematic development.
- Buildings are directly interfacing the street.
- Clear base, middle and top elements distinguished in the architectural materials used.
- Wide pavements with trees planted between parallel parking bays on the street.



© Google street view

Fairfax Mews

Post War Inner Suburb

Tertiary typology

- Mixed residential apartments and houses.
- Tall buildings of medium-density residential buildings.
- Contemporary buildings with some balconies overlooking the street.
- Buildings directly interface with the street with no setback or a small planted hedge.
- Narrow street with pavement and parallel parking bays on the street.
- Green space in the centre of perimeter block.



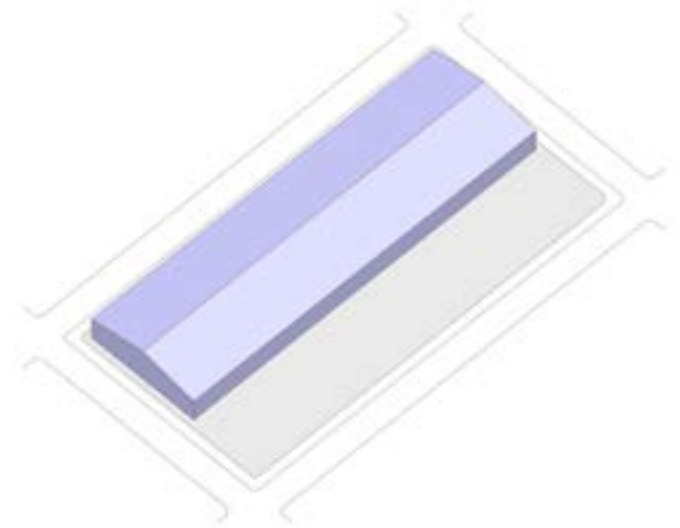
© Google street view

Admiralty Avenue

Urban Neighbourhood

Summary

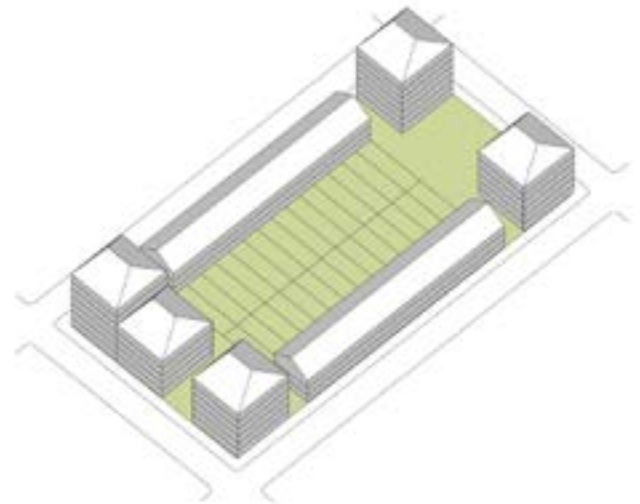
- Single storey
- Low density
- Single large building on a plot.
- Car parks in front of building.
- Approximately net 0.5 FAR



Typical warehouse block

Summary

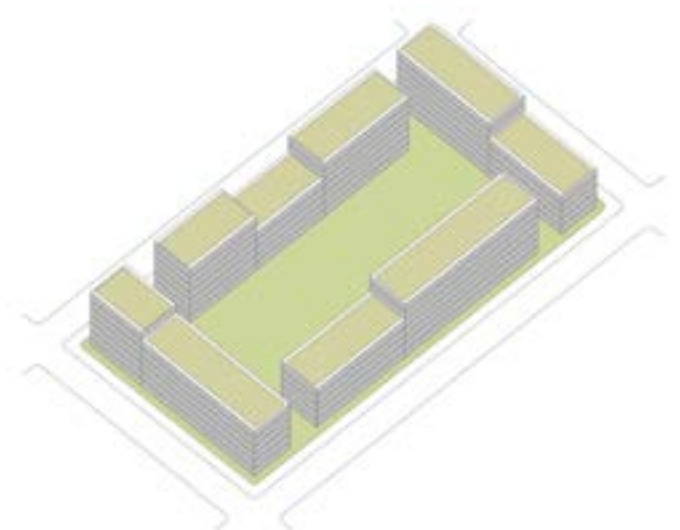
- Average 4-5 storeys
- Medium density
- Pitched roofscape
- Perimeter block development
- Central private garden and shared courts
- 1960s / 70s redevelopment.
- Approximately net 2.0 FAR.



Typical residential block

Summary

- Average. 4-9 storeys.
- Medium density
- Perimeter block development
- Flat roofscape with greening
- Contemporary architecture
- Central private shared courtyard
- Approximately net 2.5 FAR.



Typical residential block

Current and emerging urban characters

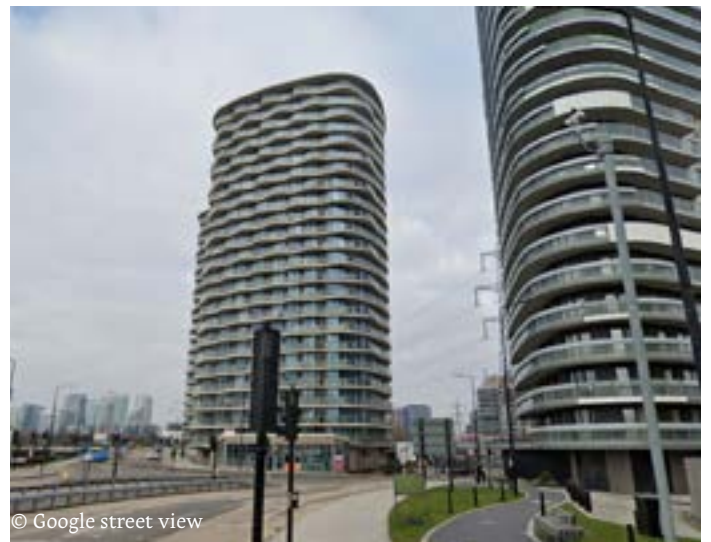
The area has seen rapid change in character created by the redevelopment of the area from former industrial lands to high density residential, hotels and office use. These typologies are spreading gradually along the old Royal Victoria and Albert dockside, and parts of the Thames including Newham Dockside (Building 1000), hotels to the east of ExCeL and new residential developments at the eastern end of the docks and around Pontoon Dock DLR station. South of Newham Way adjacent to the highway is an emerging collection of tall buildings that create a bold definition to the highway.

The typical new development consists of mid-high rise (typically 8-12 storeys, some up to 18) resulting in high densities, making use of waterside views and accessibility.



© Google street view

4-8 storey residential buildings in new developments along the Thames River.



© Google street view

Tower apartment blocks along the Western Gateway.



© Google street view

Hotels line the waters edge to the north of Gallions Point Marina

BECKTON

The character area of Beckton features largely suburban, extensive low-rise, car-centric residential development. The dominant character of the area is created by 1980s-early 1990s London Docklands Development Corporation estate housing with pockets of historic terraces at Windsor terraces. A retail park with a large Asda surrounded by landscape of at grade parking is located on the corner of Woolwich Manor Way and Tollgate Road.

The Beckton District Park is a large green space that runs north south and curves towards the east. This park is connected to King George V Park to the west which is located adjacent to Newham City farm, a small area of green space. The southern edge of Royal Road consists of another green space with little amenity. The Royal Docks Academy to the west of this green space contains some sports fields. The southern edge of this green space consists of community allotment gardens. The Beckton Green Corridor runs from the corner of Woolwich Manor Way and Frobisher Road towards the Connaught Allotments and provides additional an green pedestrian and cycling route.

The urban grain consists of a grid of curved roads, some servicing cul-de-sacs in residential areas of a medium grain. A similar but orthogonal grid exists to the western edge of the area. The urban grain also consists of segregated pedestrian routes, located at the rear of properties and are tree lined, or take the form of greenways between houses.



Churchill Road terraced housing with small private front garden and tree planted pavement.



Orthogonal street grid with largely historic housing.



Curved street grid with post war estate housing.



Retail park with Asda and large outdoor parking lot.



© Google street view

Post war housing along curved streets.

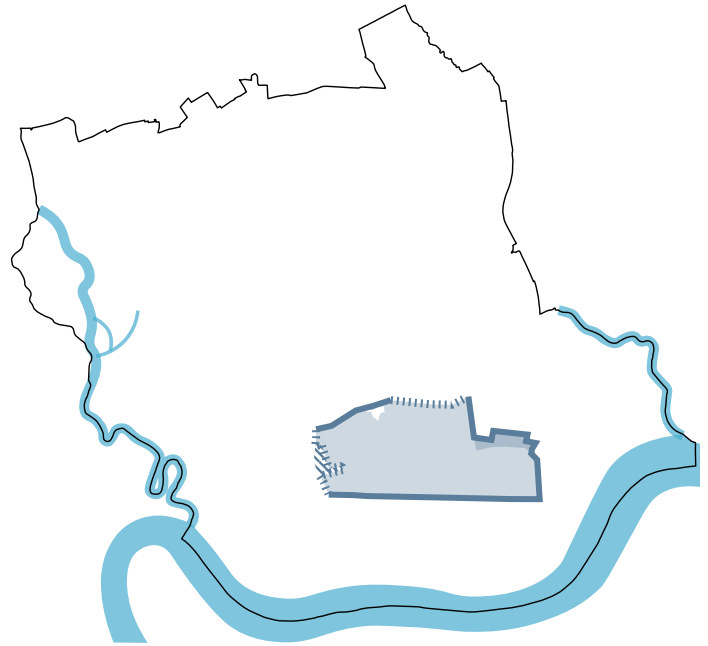


Beckton District Park.



© Google street view

Typical industrial development to the east of the character area.



- Character Area Boundary
- Proposed Extension to Character Area Boundary
- Hard Character Edge Condition
- Soft Character Edge Condition

Character Area Edges and Definition

The edges of the character area are defined by Newham Way, Prince Regent Lane, Royal Albert Way and Royal Docks Road. We have proposed an extension of the character area northwards to include the residential development between the Docklands Light Railway line and south of Angelica Drive and Henry Addington Close. The fine grain, low scale residential character of this area is more characteristic of Beckton, than the coarse grain and mixed use character of neighbouring Gallions Reach.

A soft character edge exists along Prince Regent Lane as either side of this boundary are not too dissimilar. Both sides are characterised by historic terraced housing, although the streets grids differ.

Dominant typology

- 2 storey semi-detached houses.
- Brick façades with some domestic articulation.
- Houses are set back from the street behind a low brick wall and front garden with car parking space.
- All houses have a back garden.
- Brick streets with pavement and parallel parking on the street.



Remington Road.

Post War Inner Suburb

Secondary typology

- Brick and timber-clad detached houses.
- Housing – reflects a period of fast-paced state-led systematic development.
- Houses are set back from the street behind a low brick wall and front garden with car parking space.
- Streets with dropped pavement throughout and parallel parking on the street.



Young Road.

Post War Inner Suburb

Tertiary typology

- Brick detached houses and low-rise apartment buildings.
- Housing – reflects a period of fast-paced state-led systematic development.
- Buildings are set back from the street behind a low brick wall and front garden with car parking space.
- Narrow streets with parallel parking interspersed by large trees.

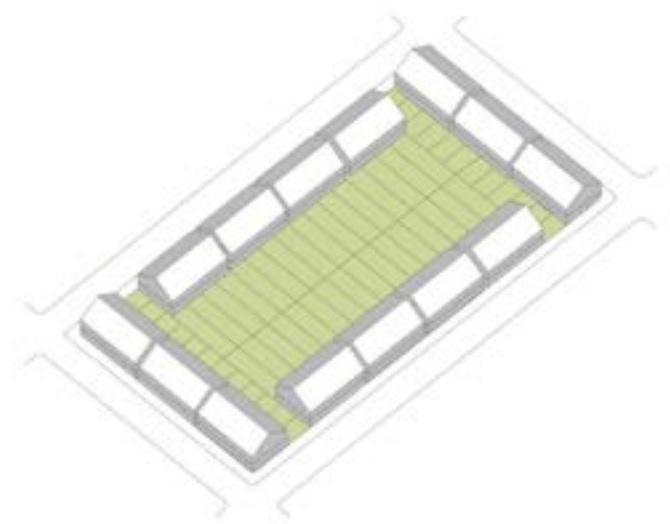


Beryl Avenue

Post War Inner Suburb

Summary

- 2 storeys
- Low density
- Semi-detached houses.
- Perimeter block development
- Domestic environment.
- Pitched roofscape with terracotta tiles.
- Approximately net 1.0 FAR.



Typical houses

Summary

- Average 2-3 storeys
- Low density
- Pitched roofscape with usable attic space in the roof.
- Perimeter block development
- 1960s / 70s redevelopment.
- Approximately net 1.2 FAR.



Typical residential block

Summary

- Average 3 storeys
- Low to medium density
- Pitched roofscape with liveable attic space in the roof.
- Perimeter block development
- 1960s / 70s redevelopment.
- Approximately net 1.0 FAR.



Typical semi-detached residential housing

Current and emerging urban characters

The majority of the existing housing stock has not changed or been redeveloped since it was first built but there are emerging residential developments to the north of the character area along Newham Way.

Beckton Parkside is a contemporary residential development being built that consists of 4-8 storey buildings.



New residential development at Beckton Parkside along Newham Way.

GALLIONS REACH

The Gallions Reach character area consists of riverside uses occupied by large scale utilities infrastructure, transport infrastructure, industrial warehouse development, Gallions Reach retail park and some emerging residential development in the south along Armada Way.

The area has a significant and long-standing role for development of utility infrastructure. In Victorian times it was the site of an extensive gas works plant and also the location of the eastern outlet of Bazelgette's sewer system, which later became the Sewage Treatment Plant that now covers large parts of the northern half of the area and serves a strategic city-wide role.

The dominant character is created by the industrial warehouses, sheds and big box retail units occupying large portions of the area. This layout is oriented towards the car and this results in a number of 'left over' green spaces without and defined role or function. The urban grain of the area is therefore extremely coarse and inaccessible to pedestrians. The roads that exist are curved, with many roundabout intersections and little to no accessibility for walking or cycling.

The green space in the area benefits from the Beckton Creekside Nature Reserve designation which encompasses part of the marshland along the River Roding. However, the theme of pedestrian inaccessibility continues with only one walking path through, itself only accessible off Jenkins Lane.



© Google street view

The area is characterised by road infrastructure with little pedestrian or cycling infrastructure



Beckton Creekside Nature Reserve pathway.



© Google maps

Mix of out of town retail and infrastructure sites create a coarse and illegible street network.



© Google street view

Out of town retail surrounded by large parking area



© Google street view

Gallions Reach retail park is a major employer and destination for those inside and outside of the borough.

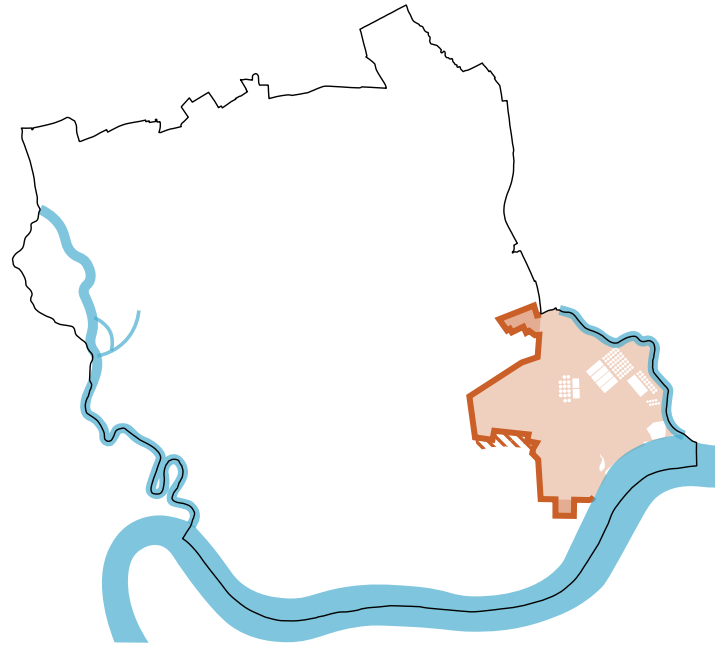


Beckton District Park.



© Google street view

Typical industrial development to the east of the character area.



- Character Area Boundary
- Proposed Extension to Character Area Boundary
- Subtracted parcels from LB Newham Extension to Character Area Boundary
- Hard Character Edge Condition

Character Area Edges and Definition

The edges of the Gallions Reach character area are well defined and clearly contains industrial and infrastructure typologies. The character area has been extended northwards to include the Folkshore Road Depot.

The boundary was amended at two points. In the south the boundary now reflects the large footprint industrial building and surface car park south of Atlantis Avenue itself more akin to the character of Gallions Reach than the high density residential character emerging in the east of Royal Docks character area. In the west, the boundary now returns low scale, Historic Inner Suburb and Post War Inner Suburb housing along Windsor Terrace - more characteristic of Beckton than the coarse urban grain, industrial and big box retail character of Gallions Reach.

Dominant typology

- Beckton Sewerage Treatment Works.
- Built in 1864 and covers over 100 hectares
- Large circular and rectangular water and filtrations tanks
- Green space between tanks
- Surrounded by a wetland park along the River Roding edge to the north and east.



Beckton Sewerage Treatment Works.

Major infrastructure

Secondary typology

- Large industrial warehouses.
- Low density industrial buildings.
- Logistics and storage warehouse space.
- Buildings are set perpendicular to the street to allow off street between buildings. Building location results in many blank faces on the street edge.
- Poor pedestrian and cycling environment.
- Streets have good tree planting and some green verges.



Eastbury Road.

Industrial

Tertiary typology

- Out-of-town-retail
- Low density industrial buildings.
- Buildings are set back away from the street and surrounded by large swathes of at grade parking.
- Poor pedestrian and cycling environment.
- No pedestrian and cycling entrance.

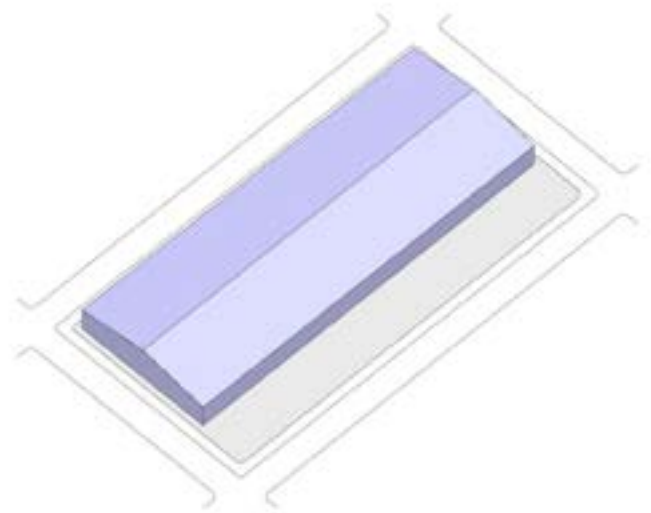


Beryl Avenue

Out of town retail

Summary

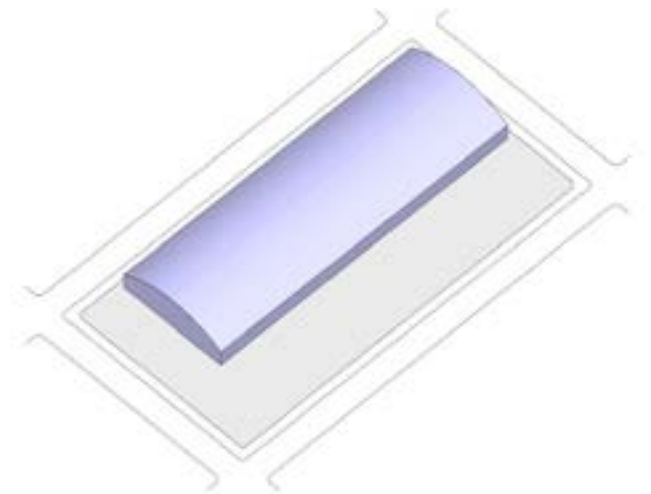
- Single storey
- Low density
- Single large building on a plot.
- Car parks in front of building.
- Approximately net 0.75 FAR



Typical warehouse block

Summary

- Single storey
- Low density
- Single large building on a plot.
- Car parks surrounding building.
- Approximately net 0.4 FAR



Typical semi-detached residential housing

Current and emerging urban characters

Emerging contemporary residential development on the south end of Armada Way on the border of the Royal Docks character area shows an indication of the need for housing in the area. The current development is an apartment typology of 5-12 storeys.



Emerging contemporary residential development at the border of the area with the Royal Docks - Armada Way

UPTON, EAST HAM, WEST HAM & PLAISTOW

The character area is dominated by historic terraced housing, with social amenities such as libraries, churches and schools. The retail offer is mostly in the form of a number of local and town centres focused on main roads with predominantly independent shops and other businesses reflecting multi-cultural diversity in the area.

There are also some areas of post-war housing estates including walk-up blocks, tower blocks and 1970s houses built into the older fabric.

The urban grain consists of a network of medium / fine grain orthogonal gridded street structure, which support long and narrow perimeter blocks. There are some pockets in which the urban grain loses its orthogonal structures, specifically towards Canning Town and around post-war housing estates such as the estate adjacent to Cleves primary School.

The area contains a number of conservation areas and buildings, notably the Woodgrange Estate, examples of Georgian houses on Romford Road, the Technical Institute and Museum (now University of East London), built in 1895 (Grade II*). A number of historic public houses also exist in the area, such as the 16th Century Spotted Dog (Grade II), and the King Edward (early 18th Century, listed Grade II). The area also contains a number of important parks, including West Ham Park, Central Park and Manor Park Cemetery and the City of London Cemetery (a grade I Historic Park) which are well connected to its surrounding residential urban structure.



Long and narrow rectangular street grid with terraced housing.



Post war housing including walk up flats and towers.



Examples of Georgian houses on Romford Road.



© Google street view
Typical historic terraced housing - Whyville Street



© Google street view
Wide and planted pavements and parks surrounding post war tower blocks and walk up flats.



© Google street view
16th Century Spotted Dog (Grade II listed) on Upton Lane.



- Character Area Boundary
- Proposed Subtraction from LB Newham Character Area Boundary
- Hard Character Edge Condition
- Soft Character Edge Condition

Character Area Edges and Definition

The northern and eastern edges of the character area are defined by the edge of the borough boundary. The hard edges along the south eastern edge of the character area represents a change in character from industrial to residential, whereas the area interfacing the Beckton character area is less defined in terms of character as both sides of Newham Way consists of 2 storey terraced housing. The western edge of the character area is well defined by the railway line parallel to Bridge Road running north towards the A12.

The boundary was revised in the west to reflect threshold where the pattern of compact, residential Historic Inner Suburbs is disrupted by leisure and education uses including Cumberland Community School and Newham University Hospital, themselves representing a coarser and more loosely arranged urban grain. The boundary was amended in the east along the brook bordering Folkestone Road and Langdon Academy, reflecting the change from low scale, residential Post War Inner Suburb, to the coarse grain and loose arrangement of industrial sheds accessed by Jenkins Lane.

Dominant typology:

- Historic terraced housing.
- Consistent 6m wide facade.
- Buildings have small setback from the street with a stoop area behind a low brick wall.
- Large bay windows face the street.
- Some decorative elements on the facade of the buildings.
- Materials consists of a consistent brick facade.
- All houses have a private back garden.
- Gridded street layout.



Halley Road.

Historic Inner Suburb

Secondary typology

- Unembellished brick walk-up flats.
- Buildings set back from the street behind a low brick wall or low planted hedge.
- Wide pavements with trees planted between parallel parking bays on the street.
- Curved street layout with some cul-de-sac.



Richmond Street.

Post War Inner Suburb

Tertiary typology

- Mixed tall residential towers with walk up flats around the edges.
- Buildings are set back from the street behind a brick wall and access control.
- Poor pedestrian environment with not trees or landscaping in the public environment.



May Road.

Post War Inner Suburb

Summary

- 2 storeys
- Low to medium density
- Domestic architecture
- Pitched roofscape
- Perimeter block development
- 1830s 1900s architecture
- Approximately net 1.0 FAR



Typical houses

Summary

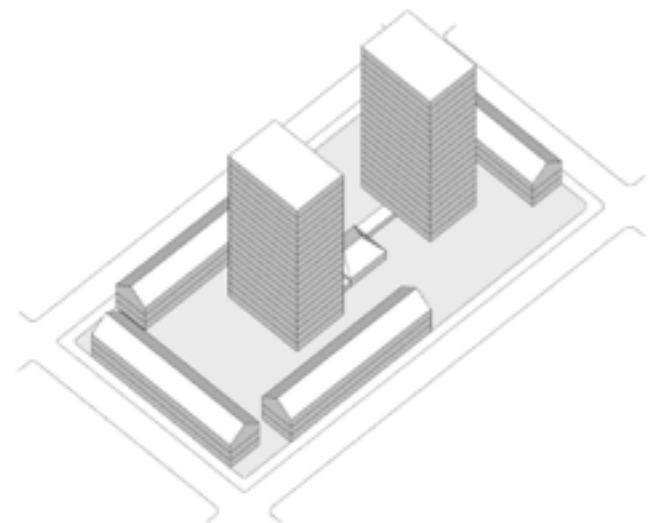
- Average 3 storeys
- Low to medium density
- Pitched roofscape
- Perimeter block development
- 1960s / 70s redevelopment.
- Approximately net 1.2 FAR



Typical residential block

Summary

- Average 14-18 storeys.
- Low to medium density
- Tall buildings
- Flat roofscape
- Late 1960s-70s architecture.
- Approximately net 1.5 FAR.



Typical tall building

Current and emerging urban characters

Recent change has included high density residential development some of which include many community facilities such as parks, leisure areas and family play area. An example of this is Upton Gardens within a local centre in Green Station.

More emerging development in the area includes the Plaistow development hub which encompasses 180 residential dwellings and community facilities a library, neighbourhood centre, supermarket, gym, café and piazza.

Recently completed Kempton Mews in East Ham represents a more efficient use of land. The backland development intensifies and increases the density of an urban terrace block, creating a more compact and intimate character.



Upton Gardens residential development.



Kempton Mews, East Ham



Plaistow Hub mixed use development.

URBAN MORPHOLOGY CONCLUSIONS

Reviewing and analysing the urban morphology and built character of Newham across the 7 character areas helps identify the defining identities of Newham, whilst acknowledging the emerging characteristics.

Urban morphology and physical form provide a very tangible way in which to define character, one that many can immediately relate to. In this way, it can also be a useful tool in helping to define the identities of Newham, as well as where neighbourhoods sit and where their edges are. The assessment is of a particular point in time, but importantly the assessment has also included a review of the emerging characteristics. It is clear that some areas have gone through significant change already – such as around Stratford; whilst other areas are witnessing ongoing change – such as around Canning Town; and other areas are witnessing an emerging character – such as the Royal Docks. Meanwhile much of the borough looks to have maintained a consistent character that is unlikely to see significant change in the near future – such as around East Ham.

The new neighbourhoods proposed later in this report are informed by this assessment of current, emerging and new identities. Current identities can be described in part through urban morphology, and for those undergoing change these can look at the degree to which character may continue to change in each neighbourhood – taking into account the degree of change recently and acknowledging the likelihood of further changes or new changes. This gives cues as to the appropriate degree of intensification.

In describing edges of character, built form is often tangibly apparent, with changes in morphology creating distinct differences between areas. Where Canning Town character area meets the River Lea character area there is an evolving boundary based on current development patterns; as is also the case where Gallions Reach character area meets the Royal Docks character area. Again, the neighbourhood boundaries can be partially defined by these, particularly where changes in form are significant. Whilst the character areas reflect the current nature of these edges, evolution and expected change can inform how neighbourhood boundaries should account for future character. Given that these edges are in flux in places, the evolution of these is important to consider when defining neighbourhood boundaries, taking on board the opportunity to define how the extents of different built characters can be sculpted.



STRATFORD CENTRE

STRATFORD CENTRE